



Town of Hull

Community Development & Planning
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Public Participation Concept Plan for Nantasket Beach Revitalization

Thank you for coming to the website to participate in shaping the future of your community. This is an opportunity for you to shape a future that is healthy by design by providing input into the creation of a concept plan to guide future redevelopment. At this site you will see the consultants 2nd DRAFT Concept Plan presentation from the January 8, 2015 public meeting. You will have additional opportunities to provide your input to Town Officials and the Hull Redevelopment Authority. After the second meeting a third meeting time and place will be determined.

Let me be clear that what you see in the consultants PowerPoint posted here and presented on January 8, 2015 is not what will actually be built. It is a guide only. It will guide development to what you want for your future and that is why it is so important to get your ideas and input. In the consultant's presentation you will see blocks which represent buildings with no architectural detail. This is just a step in the design process. You will see another slide with examples of architecture which are typical of the historic architecture of the Town. These examples are also consistent with the Nantasket Beach Overlay District and will be required in future development proposed under that zoning. Also be assured that the Overlay reduces building massing and protects open space and view corridors.

Thanks again for participating in this process and making this revitalization the best it can be.

Best Regards,

Robert L. Fultz, MPA
Director

Hull, MA

Nantasket Beach Revitalization Plan

Draft Redevelopment Scenario





Why Revitalize the Nantasket Beach Area?

What do we believe?

What is our goal?



Consensus Goal from 2011 Community Workshops

“To maximize the town’s commercial tax base by supporting an environmentally and commercially sustainable development model which serves as a catalyst for a successful economy and recognizes that Hull’s historic character and natural beauty and resources are fundamental (to success).”

Economic Revitalization

Help meet Community needs - better balance tax burden; meet \$60 M in capital needs.

“**Best place to start revitalization**”

2004 – Town Plan

How Are We Revitalizing Nantasket Beach?



Developing Partnerships and New Consensus

Community Participates / Town /HRA / MassDevelopment / Hull Chamber / South Shore Chamber / Metropolitan Area Planning Council / MassWorks / DCR / Representatives

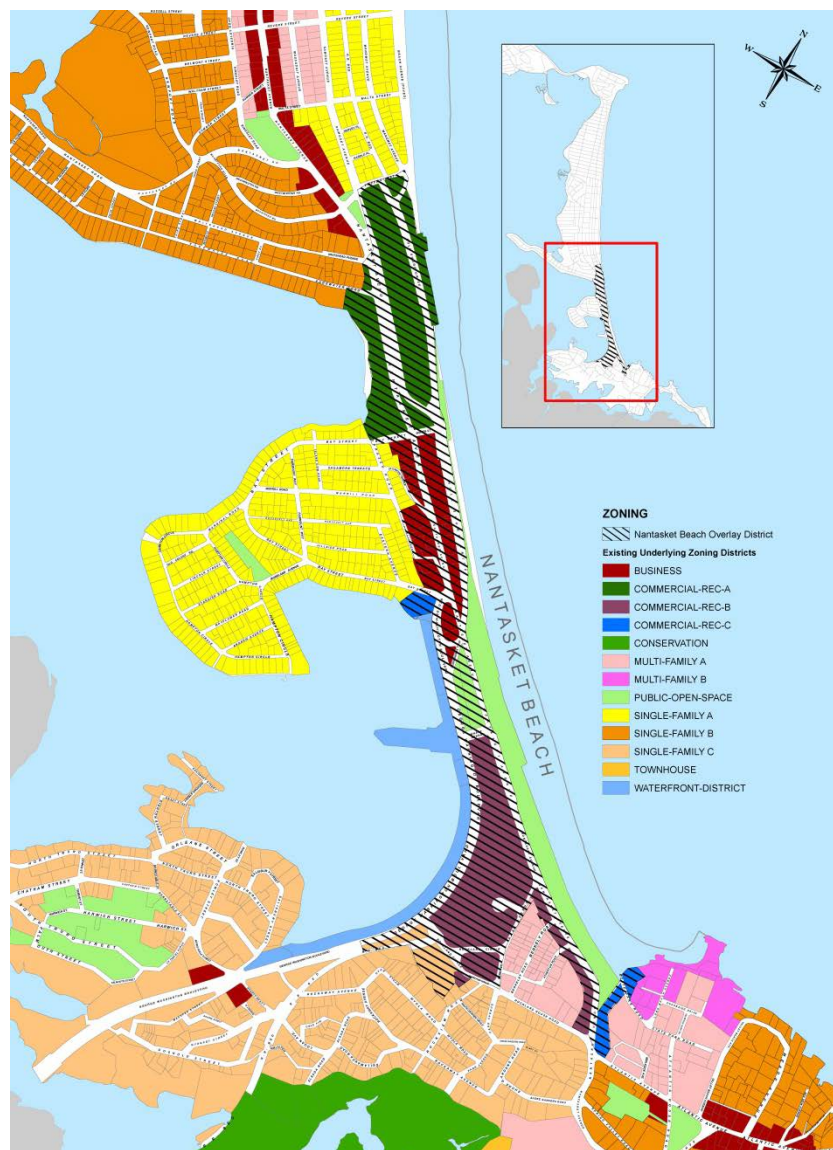
Supporting Existing and New Ventures

Rebuilding infrastructure, providing clear direction and new tools for re-development



What Has, Is and will Be Done To Create the Framework for Revitalization?

Framework for Nantasket Revitalization



2011 – Goal

DCR Re-Use Committee conducts Community Workshops and defines revitalization goal and planning area.

2012-2013 – Zoning

After 23 public meetings the community creates the Nantasket Beach Overlay Zoning District, “Healthy by Design”.

2012 – 2016 - Infrastructure

HRA provides engineering/admin funds for \$1.95 M MassWorks Grant to rebuild Nan Avenue at “Surfside”.

2014-2015 - Study of two-way street system

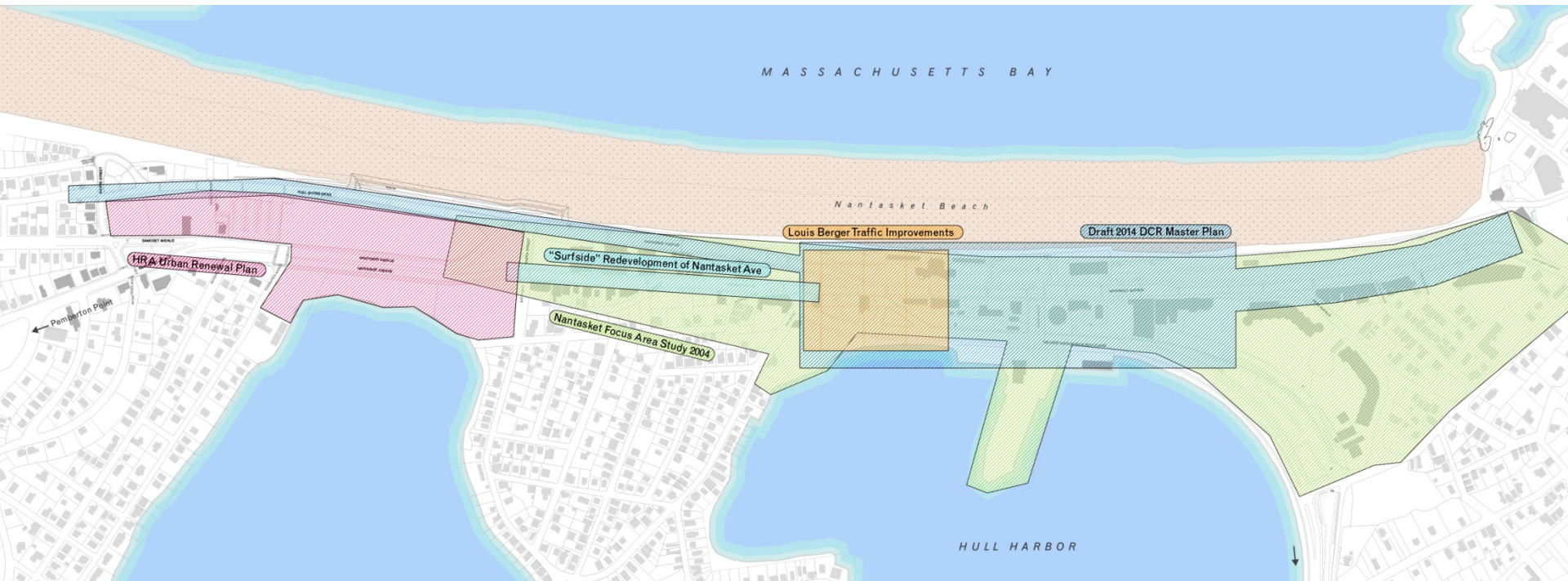
To relieve congestion at bottlenecks while supporting economic development, HRA / DCR committed funds.

2013-2015 - Provide clear guidance for revitalization

Town / HRA / MassDev hire Utile, & Nelson/Nygaard to work with the community on a Pre-development Plan to include site master plan and transportation plan, including parking.

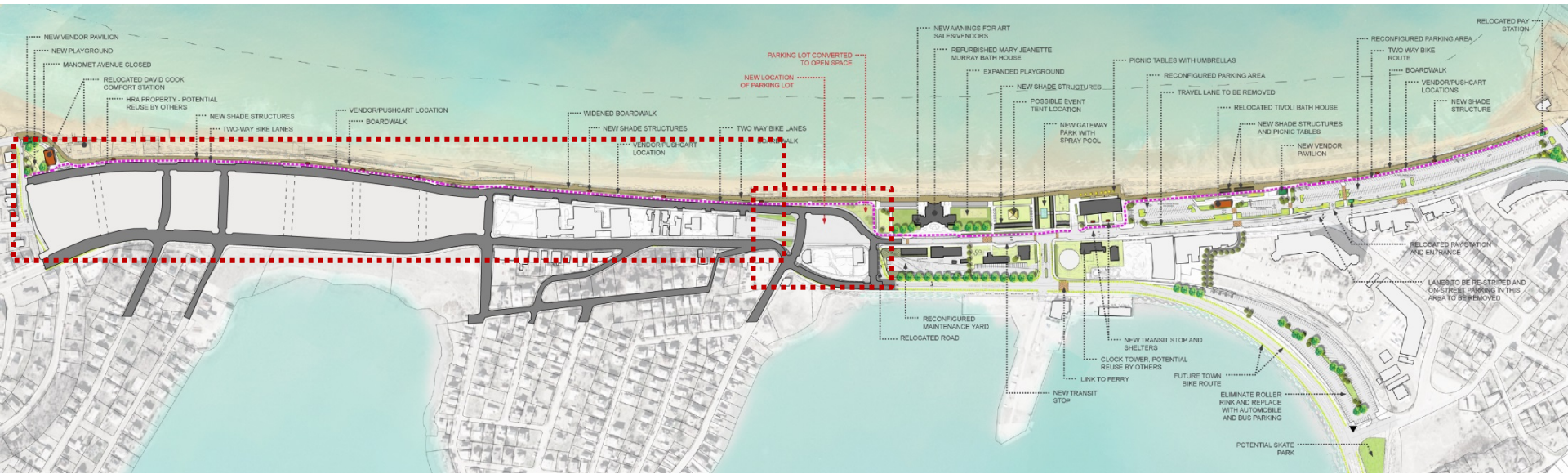
2014 – CTPS pedestrian / bicycle access study, “Livable Communities”

Recent Planning Studies for Nantasket Ave



-  HRA Urban Renewal Plan
-  MassWorks "Surfside"
-  2004 Cecil Group Study
-  Louis Berger Re-Alignment
-  2014 DCR Master Plan

Unified Master Plan



- The Town of Hull and the DCR have collaborated on studying an integrated street network.
- At sites of study area overlap (Hull Shore Drive and Anastos Corner) the street sections and road alignment are support the coordination, and advance the recommendation, of a two-way parallel road framework.
- Conceptual analysis supports taking the next step in analyzing the viability of the 2-way system, an engineering level study

Roadways Reconfigurations

EXISTING (Wharf to Water)



PROPOSED



Roadways Reconfigurations



Why 2-Way Conversion?

1. **Economic Development**
2. **Pedestrian Enhancement**
3. **Public Safety**
4. **Convenient Access**

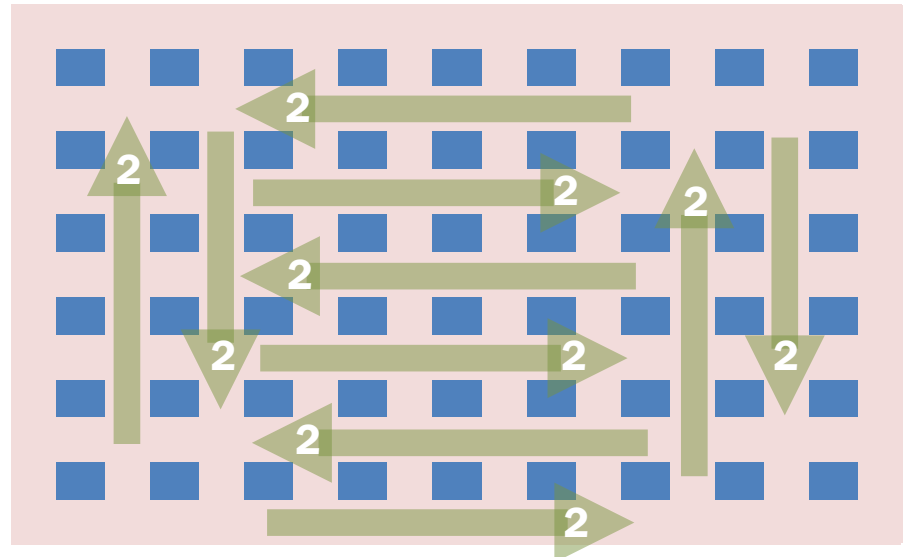
Greater access and visibility

5. **Traffic Reduction**

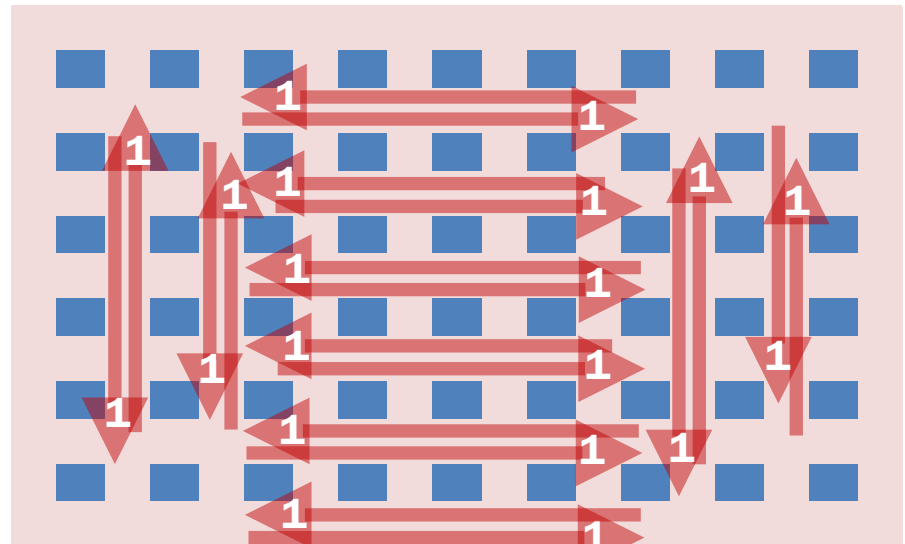
Reduce unnecessary vehicular circulation

Equal vehicular capacity to one-way network

Speed can be slower, but just as many cars get through

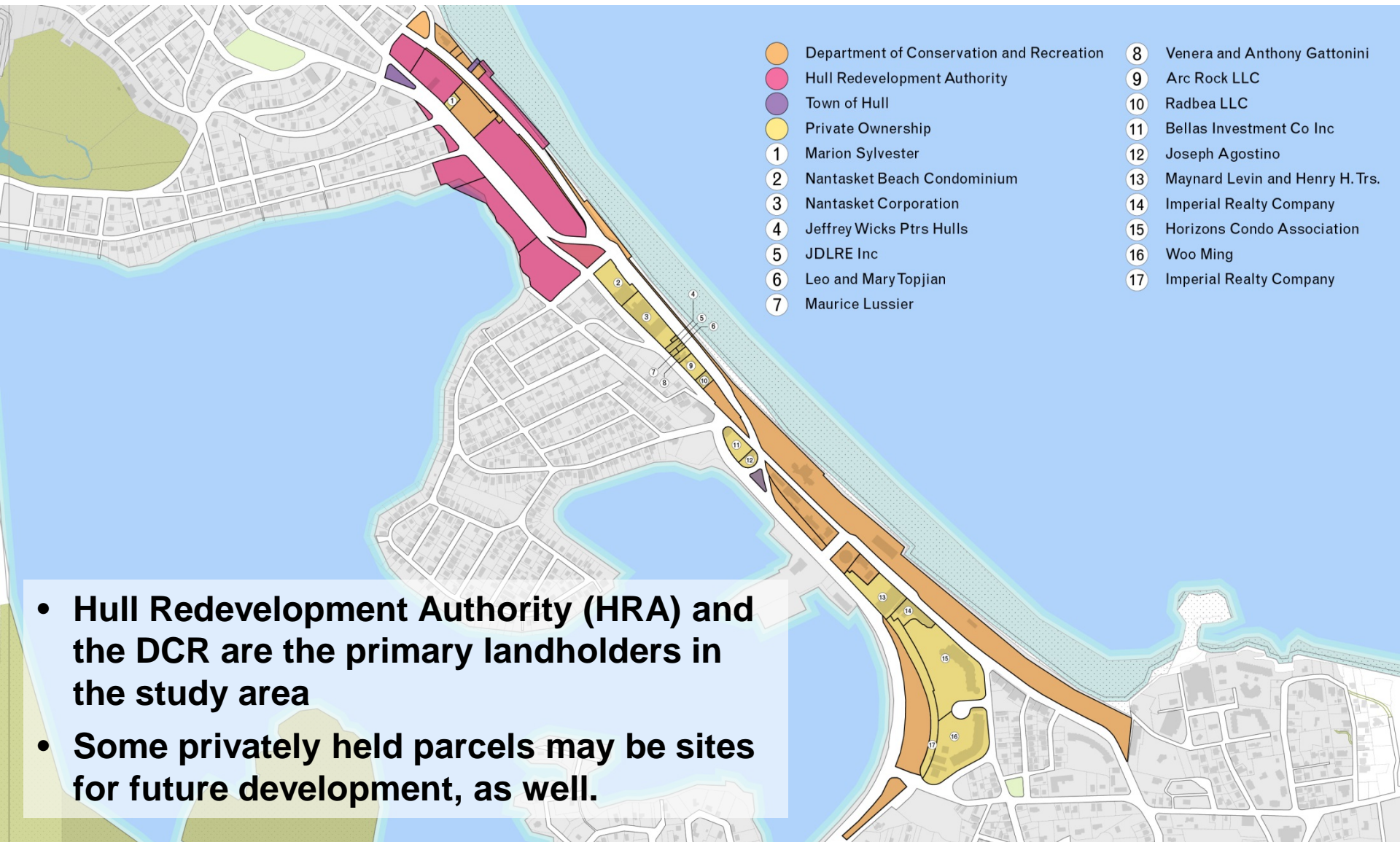


One-Way Street Grid

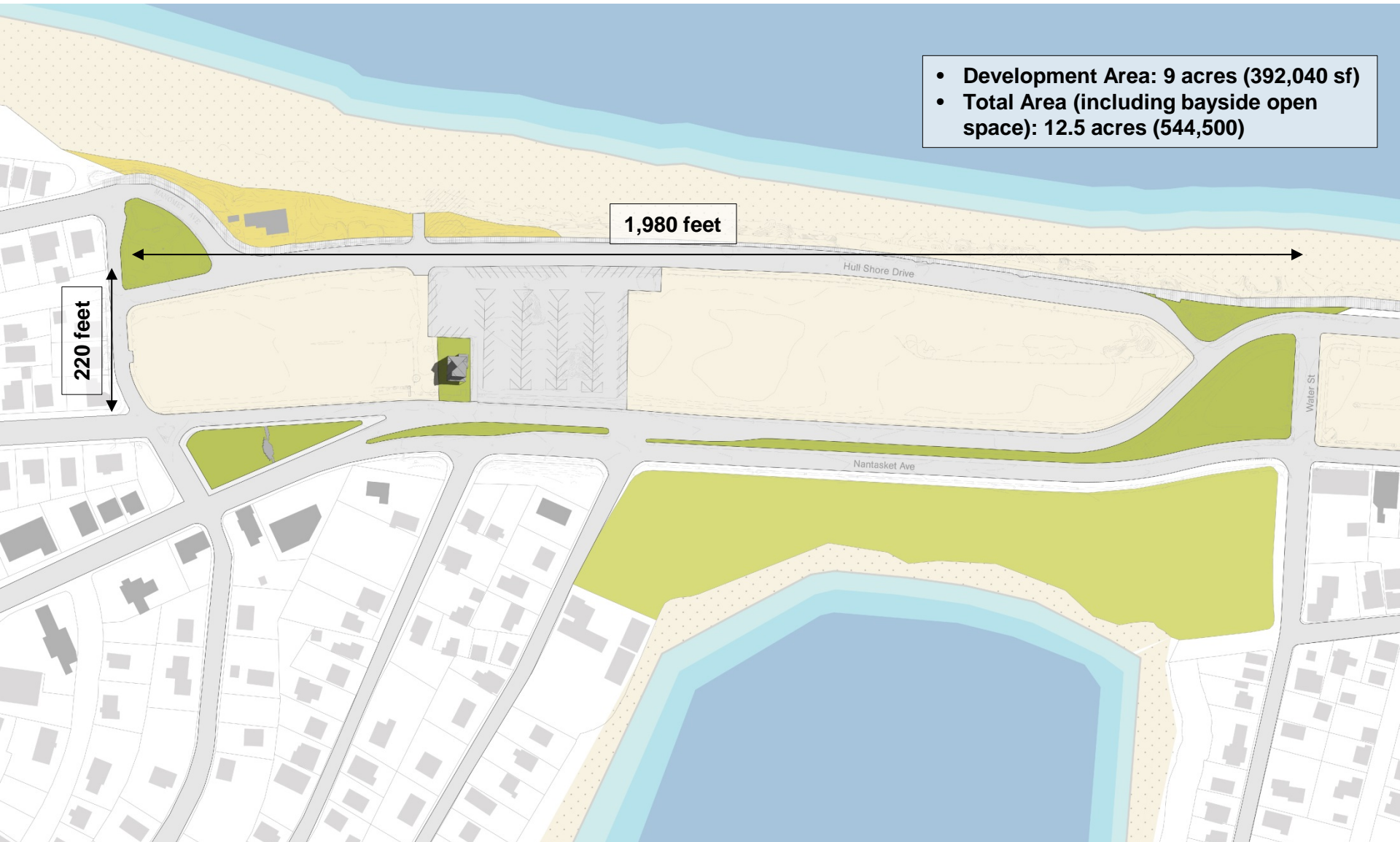


Two-Way Street Grid

Existing Ownership



Existing Conditions



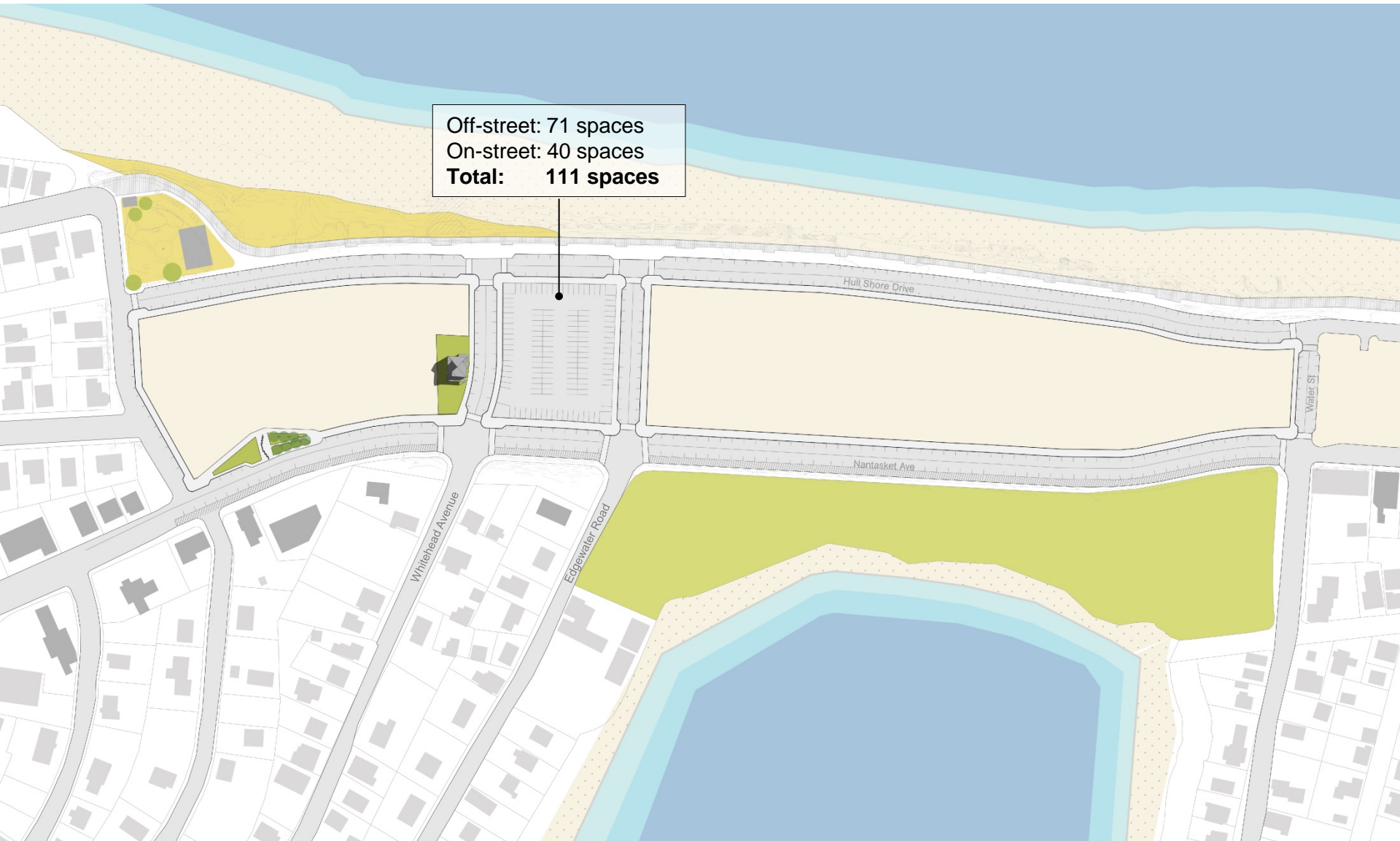
Design Principles

- Create a beautifully-scaled waterfront town center for Hull residents
- Create a unique and compelling ocean-facing retail and restaurant district that is a top destination in New England
- Create walkable blocks and streets that are patterned after the blocks to the north of the development site
- Create a development strategy that is a model for storm “resiliency” in the face of rising sea levels
- Create a mix of units that take advantage of water views and the walkable lifestyle offered by the retail boardwalk and beach
- Create a plan that can be phased over time
- Create open space is a natural and cultural resource and contributes to the resiliency of the town center

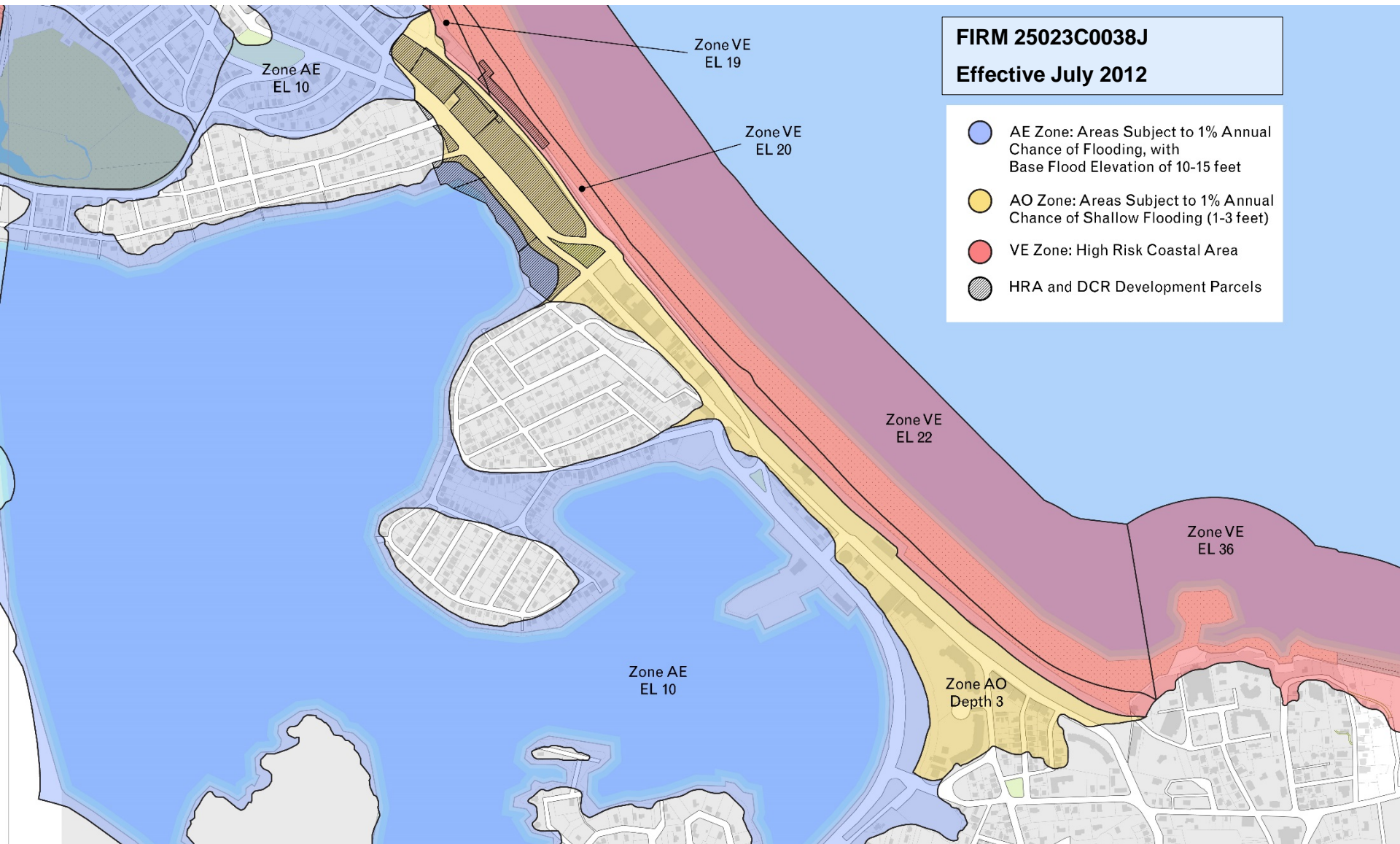
Existing Conditions



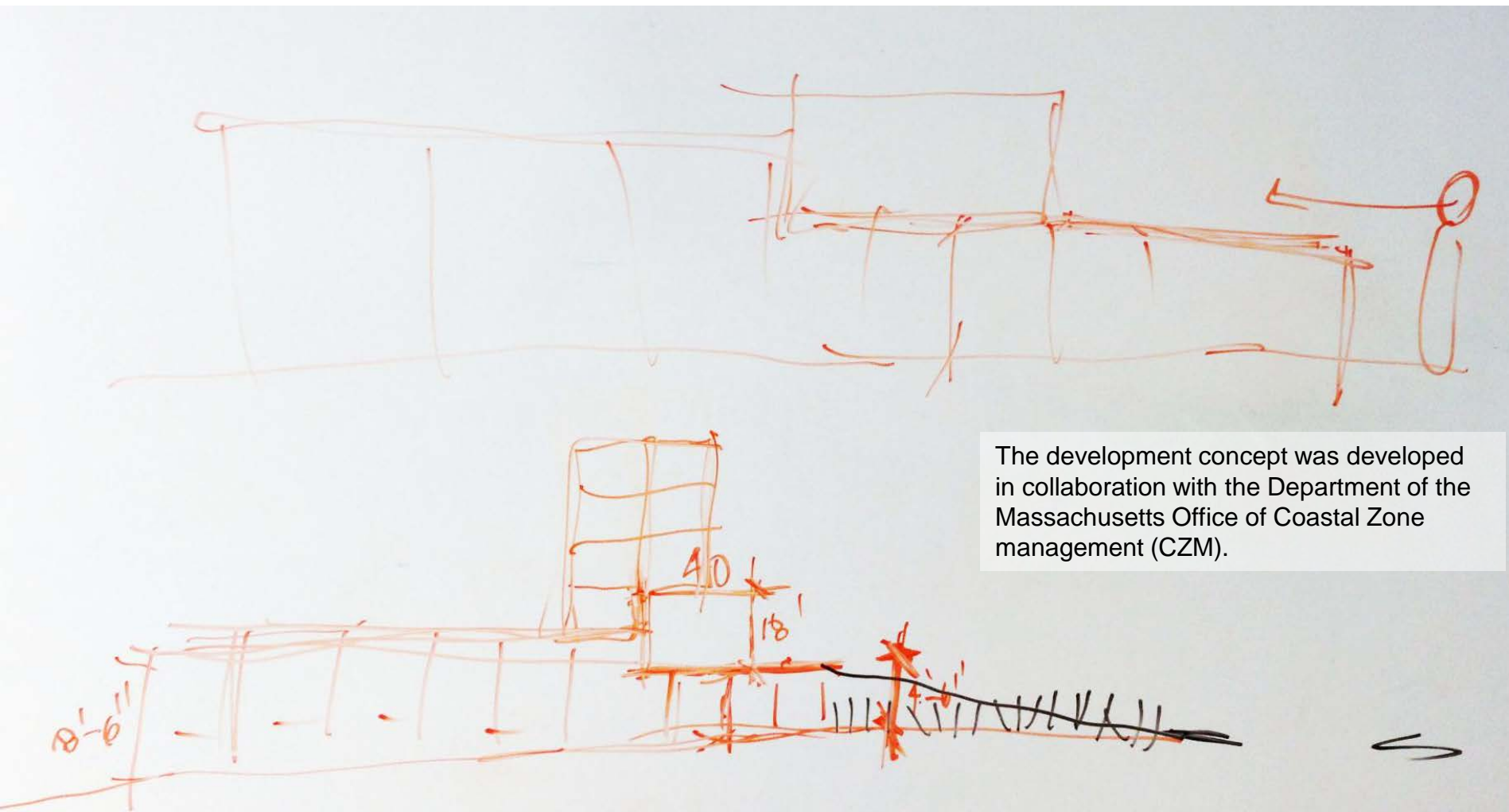
Phase One



FEMA and Flood Levels

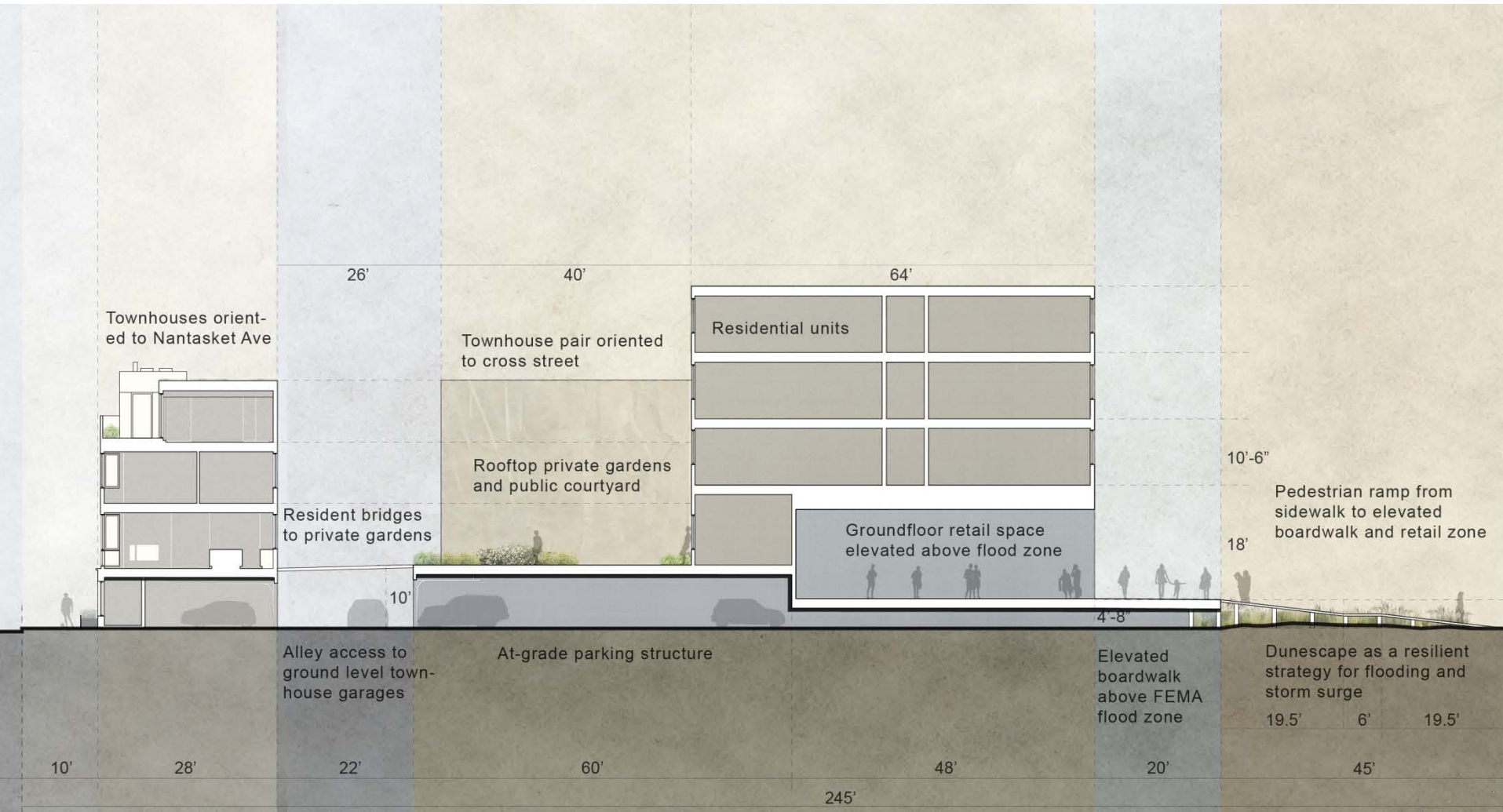


Proposed Development Section



The development concept was developed in collaboration with the Department of the Massachusetts Office of Coastal Zone management (CZM).

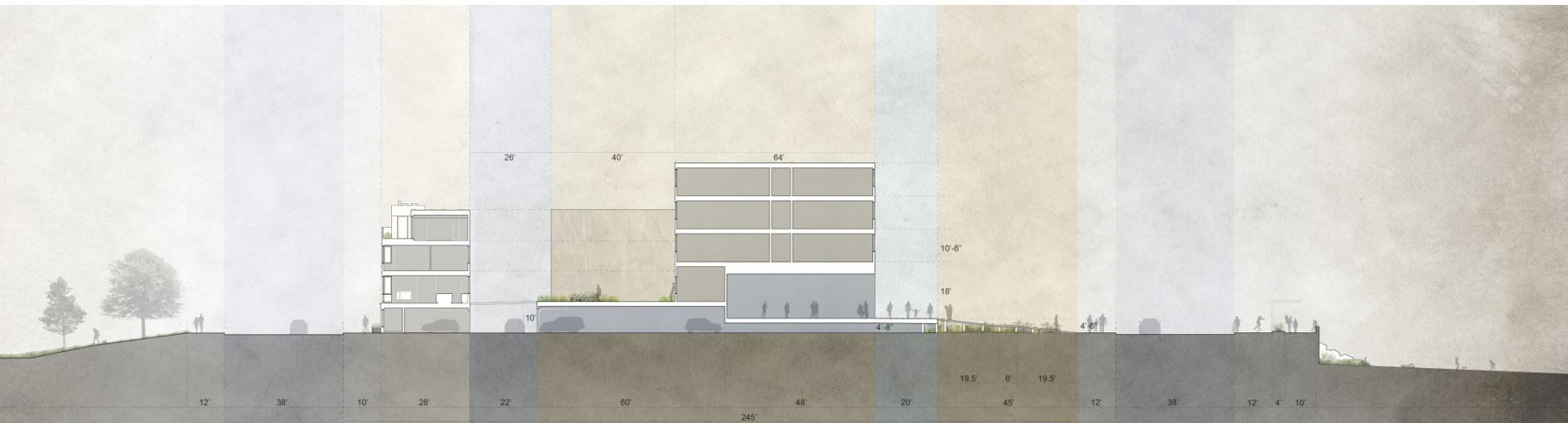
Proposed Development Section



Bayside

Oceanside

Proposed Development Section



Bayside

Oceanside

Proposed Development Concept

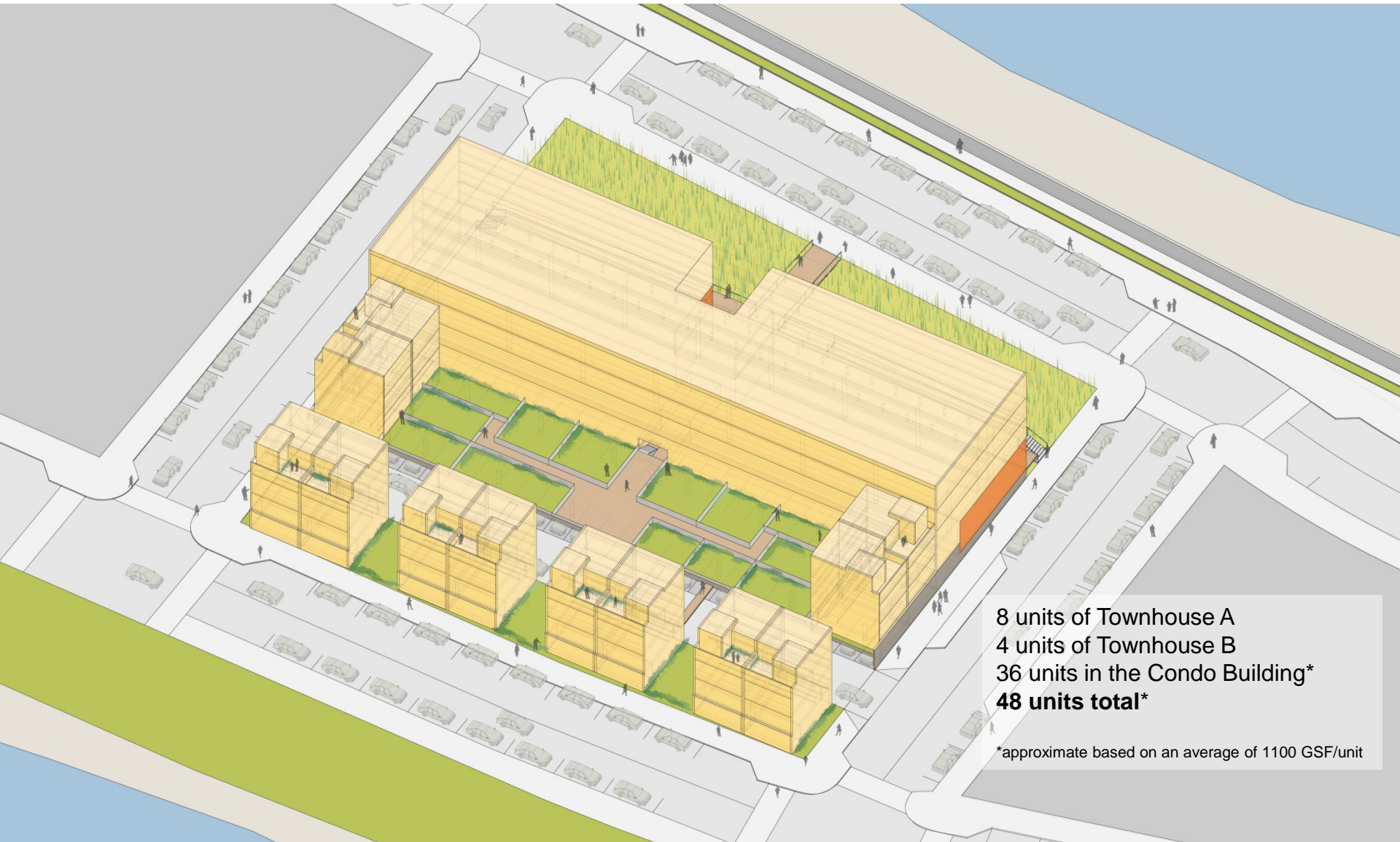


Porches, Balconies, and Terraces



Cape May, New Jersey

Proposed Development Concept



Proposed Development Concept



Proposed Wind Mitigation



Proposed Wind Mitigation



Proposed Wind Mitigation



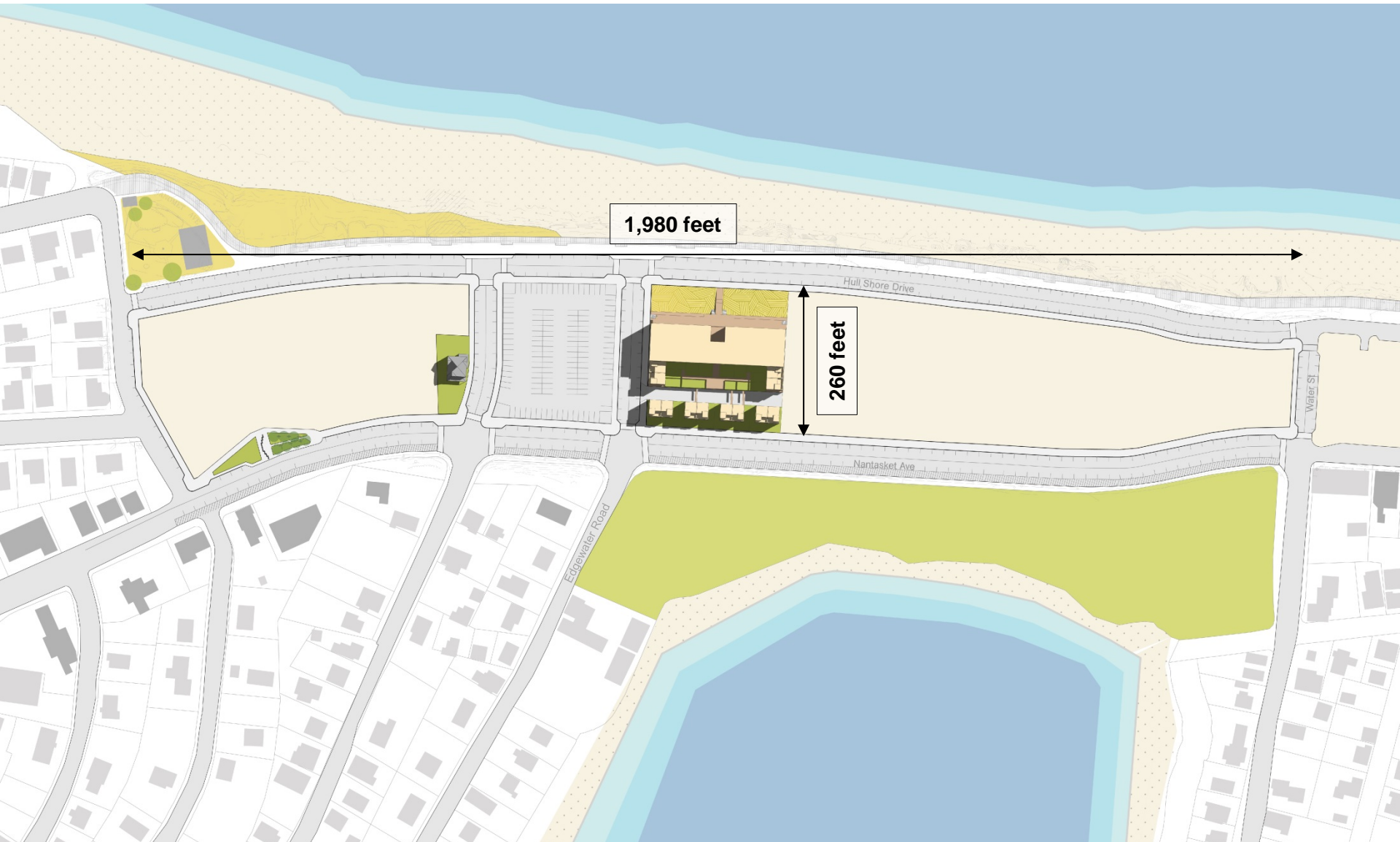
Wind Mitigation Concept



Phase One



Phase Two



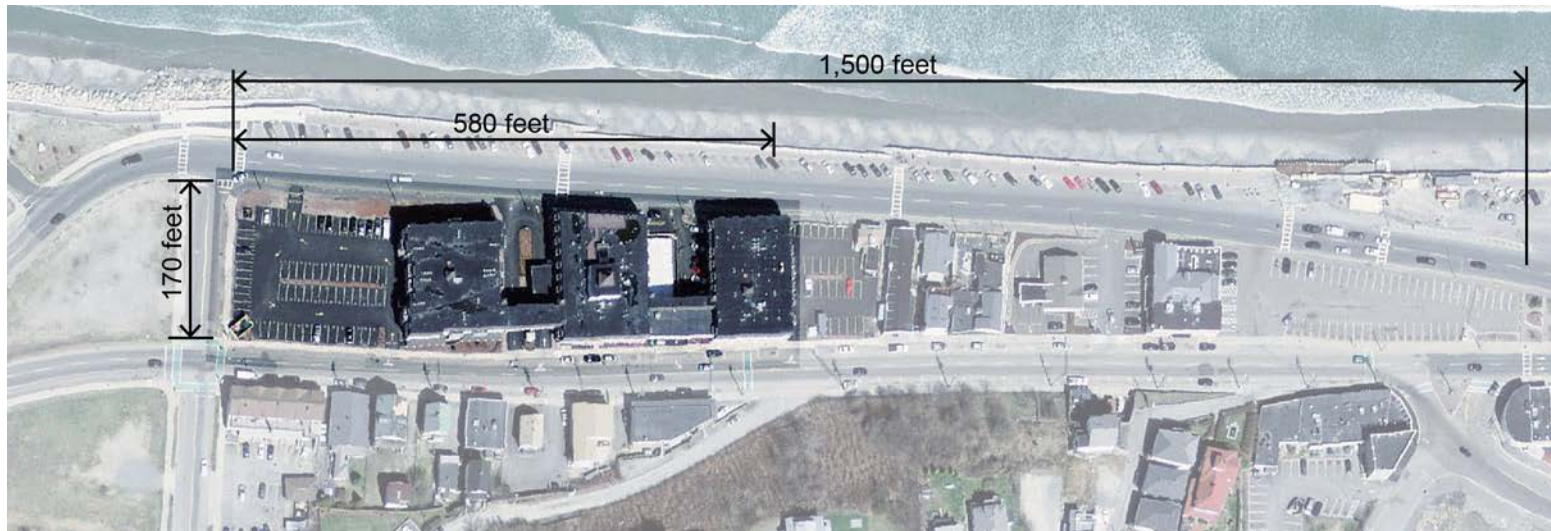
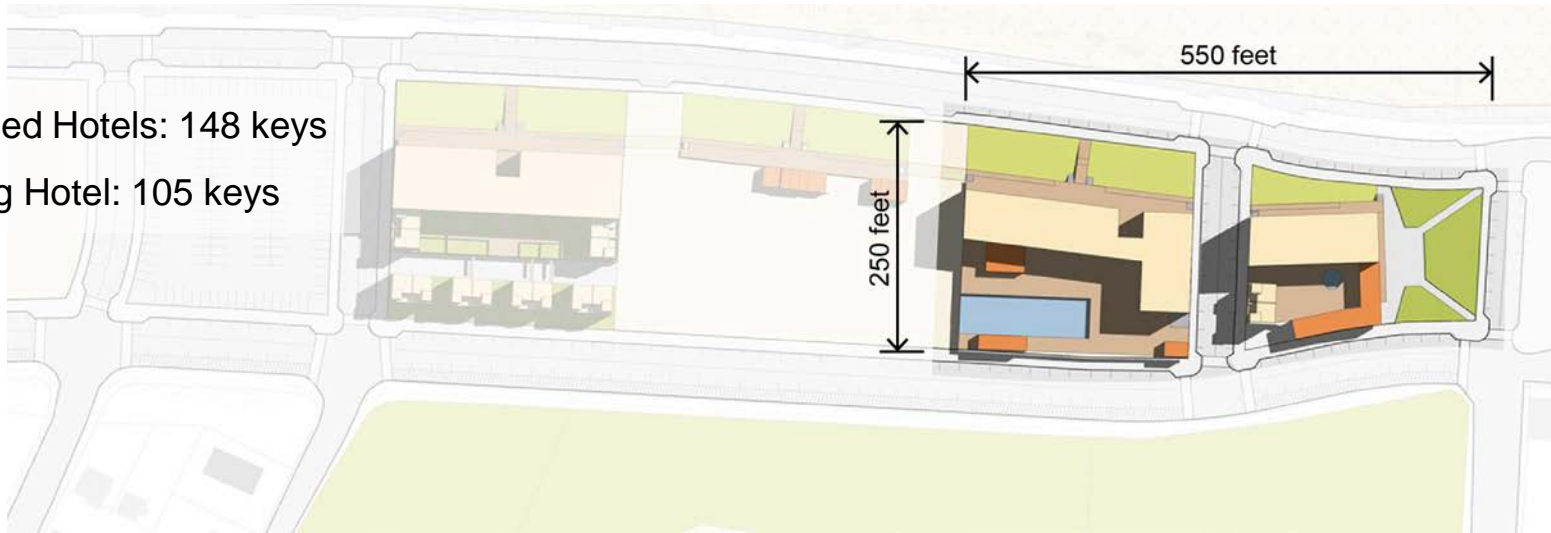
Phase Three



Scale Comparison – Hotel Development

Proposed Hotels: 148 keys

Existing Hotel: 105 keys



Phase Three



Temporary Retail Pavilions



San Francisco's Octavia Boulevard, Douglas Burnham of Envelope A+D



Google Village at Ted Long Beach, Boxman Studios

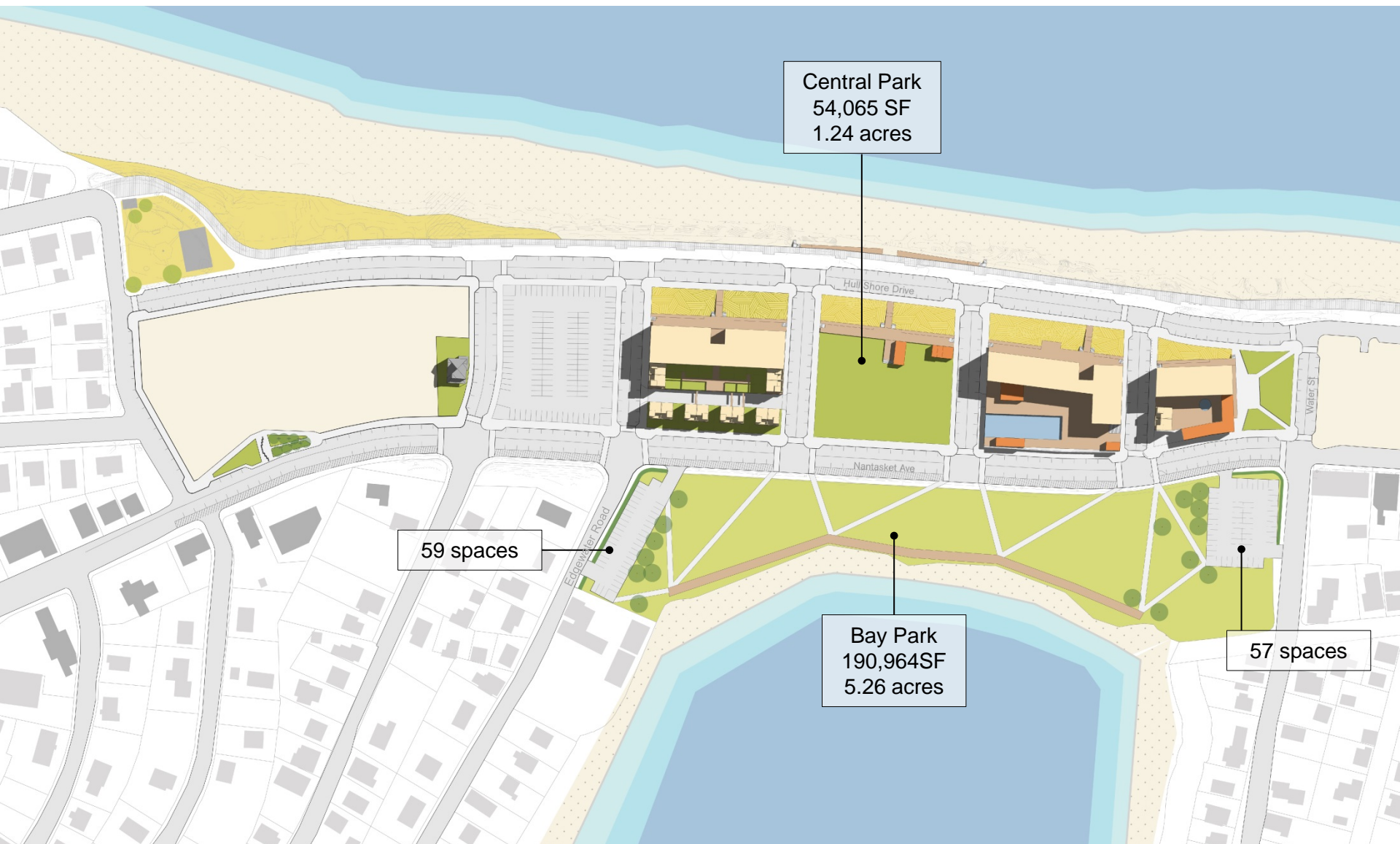
Temporary Retail Pavilions



Phase Three

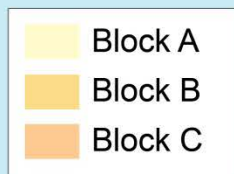


Phase Four



Why Smaller Blocks?

- Increased connectivity
- Less circuitous walking/biking/transit routes
- Facilitate mixed-use/ “park once” environment
 - Shown to reduce driving*



Proposed Blocks in Context



Proposed Development Concept



Proposed Development Concept



Proposed Development Concept



Capturing View Corridors from New Streets



Wildwood, New Jersey

Open Space Summary



Proposed Development Concept in Context



Development Concept with DCR Master Plan



Interim & Long-Term Parking Strategies



$$93 + 101 + 71 - 164 + 130 = \sim 231 \text{ space increase}$$

Proposed Development Concept

